[illegible]

SHOP CONNECTIONS: WELD  
FIELD CONNECTIONS: WELD  
HOLES:  $\frac{13}{16}$ " UNLESS NOTED  
PAINT: STATE OF ME. SPECS.

APPROVED AS NOTED 12-17-58

STRINGERS

*Bancroft & Martin Rollings Mills Company*  
*Brewer, Maine*

Essex St. Bridge  
INTERSTATE No.

BANGOR MAINE  
CUSTOMER H.B. FLEMING IN

C. DESIGNER CLARKESON ENG.

[illegible]

ORDER <u>VERBAL</u>	DWG. <u>58-2</u>
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1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	25	26	27	28	29	30	31	32	33	34	35	36	37	38	39	40	41	42	43	44	45	46	47	48	49	50	51	52	53	54	55	56	57	58	59	60	61	62	63	64	65	66	67	68	69	70	71	72	73	74	75	76	77	78	79	80	81	82	83	84	85	86	87	88	89	90	91	92	93	94	95	96	97	98	99	100	101	102	103	104	105	106	107	108	109	110	111	112	113	114	115	116	117	118	119	120	121	122	123	124	125	126	127	128	129	130	131	132	133	134	135	136	137	138	139	140	141	142	143	144	145	146	147	148	149	150	151	152	153	154	155	156	157	158	159	160	161	162	163	164	165	166	167	168	169	170	171	172	173	174	175	176	177	178	179	180	181	182	183	184	185	186	187	188	189	190	191	192	193	194	195	196	197	198	199	200	201	202	203	204	205	206	207	208	209	210	211	212	213	214	215	216	217	218	219	220	221	222	223	224	225	226	227	228	229	230	231	232	233	234	235	236	237	238	239	240	241	242	243	244	245	246	247	248	249	250	251	252	253	254	255	256	257	258	259	260	261	262	263	264	265	266	267	268	269	270	271	272	273	274	275	276	277	278	279	280	281	282	283	284	285	286	287	288	289	290	291	292	293	294	295	296	297	298	299	300	301	302	303	304	305	306	307	308	309	310	311	312	313	314	315	316	317	318	319	320	321	322	323	324	325	326	327	328	329	330	331	332	333	334	335	336	337	338	339	340	341	342	343	344	345	346	347	348	349	350	351	352	353	354	355	356	357	358	359	360	361	362	363	364	365	366	367	368	369	370	371	372	373	374	375	376	377	378	379	380	381	382	383	384	385	386	387	388	389	390	391	392	393	394	395	396	397	398	399	400	401	402	403	404	405	406	407	408	409	410	411	412	413	414	415	416	417	418	419	420	421	422	423	424	425	426	427	428	429	430	431	432	433	434	435	436	437	438	439	440	441	442	443	444	445	446	447	448	449	450	451	452	453	454	455	456	457	458	459	460	461	462	463	464	465	466
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4-DIST. 12-30-58  
4-SHOP 12-30-58  
2-FIA-12-9-58

DRAWN	12-2-58	D.
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REVISION	12-30-58	TD, A
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REVISION		
REVISION		

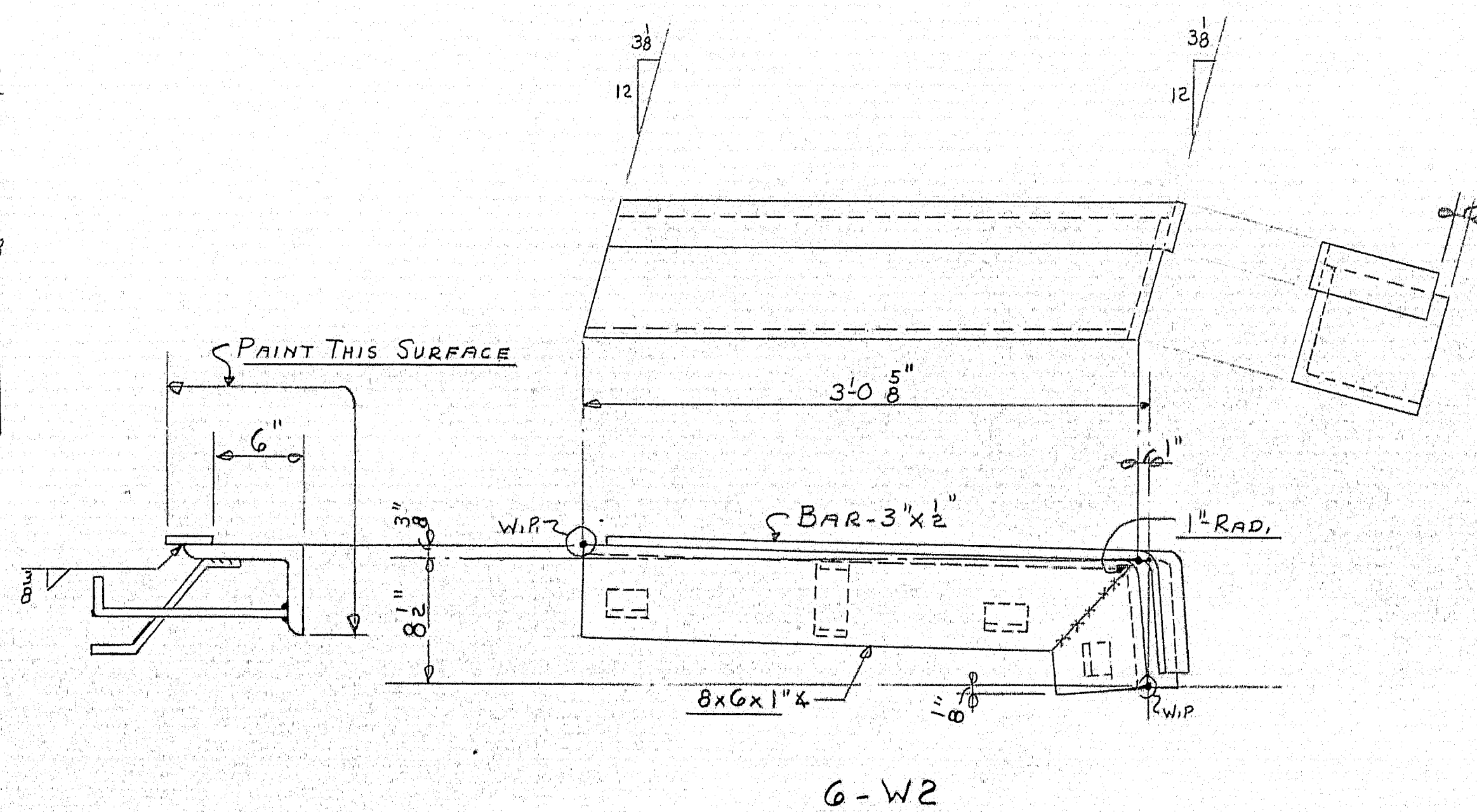
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REVISION			
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B. P. R. REG. NO.	STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	MAINE	1-95-8(5)179	12	18

## BANGOR INTERSTATE

### GENERAL NOTES

#### FOUNDATIONS:

Foundations may be altered, if necessary, to suit conditions encountered in construction.

#### DESIGN:

In accordance with the Specifications of the American Association of State Highway Officials for H20-44 loading (1953 Edition), with adopted tentative revisions.

Design Stresses: Structural Steel 18,000 psi  
Reinforcing Steel 18,000 psi  
Concrete (n=10) 1,200 psi

#### CONSTRUCTION:

State of Maine Standard Specifications to be followed except as noted in Special Provisions.

#### REINFORCEMENT:

All bars shall have deformations conforming to A.S.T.M. Designation A305.

Unless otherwise shown on plans, reinforcing bars shall be lapped 20 diameters to make a splice, except that main reinforcing bars near the top of slabs and beams having more than 12 inches of concrete under the bars shall be lapped 35 diameters to make a splice.

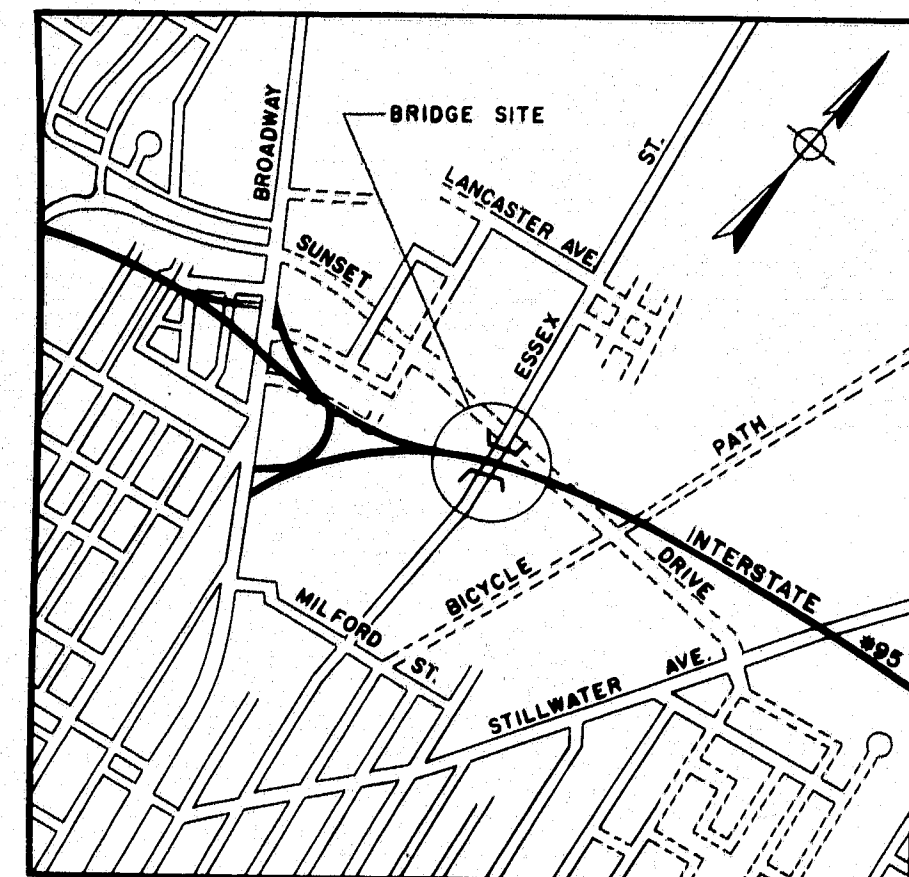
#### BENCH MARK:

B.M. S-16 Vertical "T" Bar in root of 15' Elm 0.6 mile north of Intersection of Stillwater Avenue and Essex Street, 225 feet north of road target on Essex Street  
Elevation 166.92.

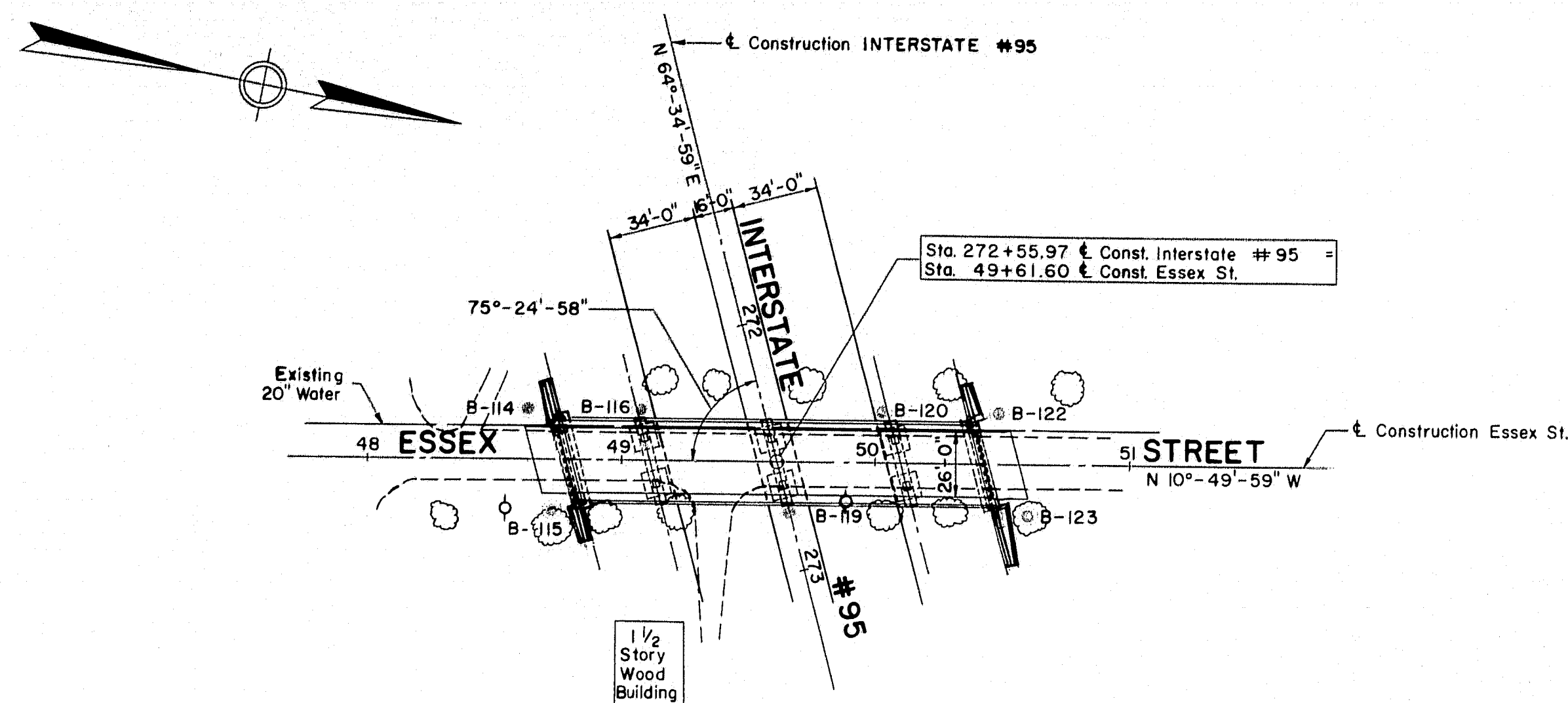
### ESTIMATED QUANTITIES

(NOT GUARANTEED)

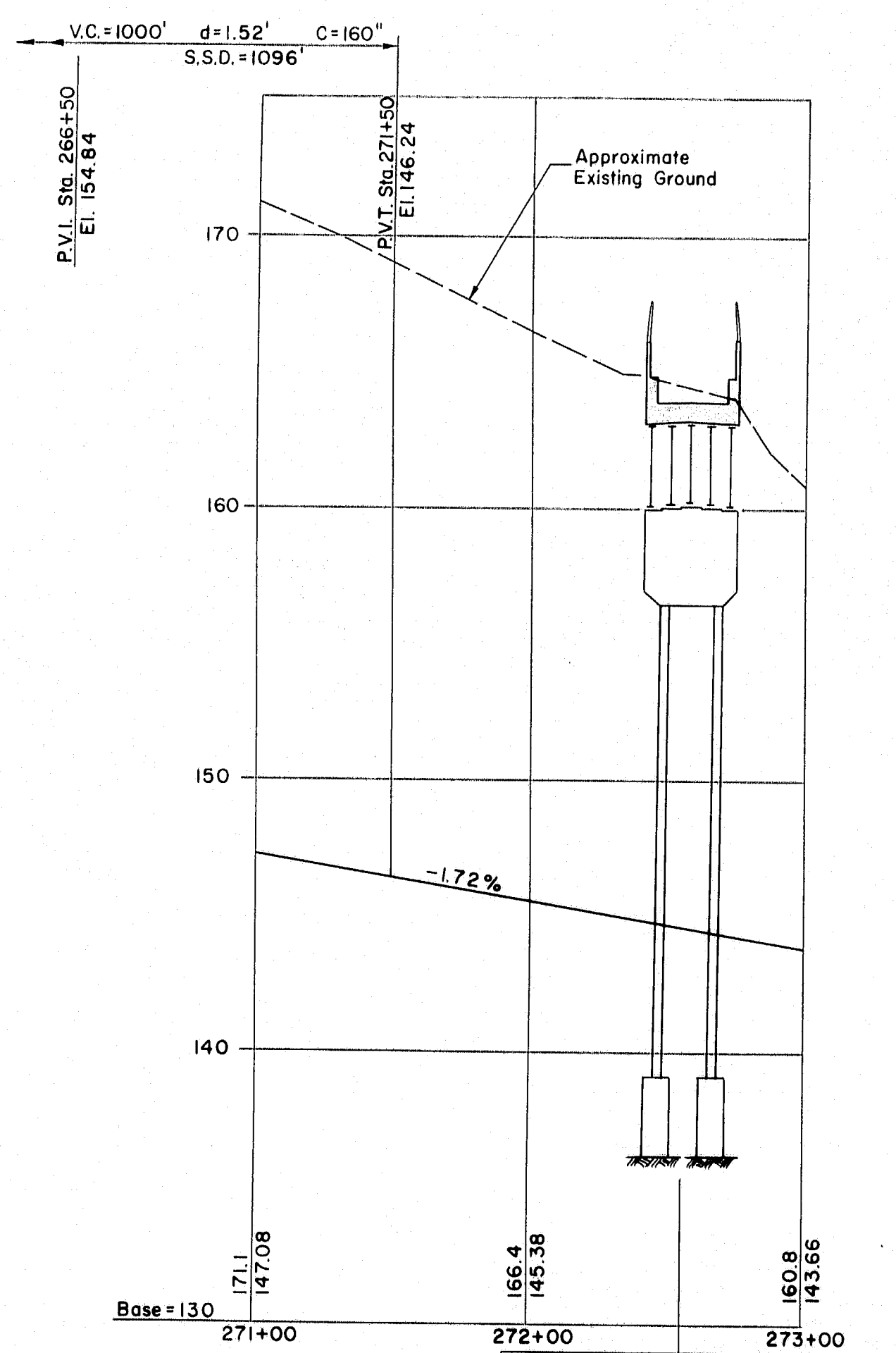
STRUCTURAL EARTH EXCAVATION, ABUTMENTS AND RETAINING WALLS.....180 CU. YDS.  
STRUCTURAL ROCK EXCAVATION, ABUTMENTS AND RETAINING WALLS.....160 CU. YDS.  
STRUCTURAL ROCK EXCAVATION, PIERS.....110 CU. YDS.  
BITUMINOUS CONCRETE SURFACE COURSE, TYPE "A".....60 TONS  
MEMBRANE WATERPROOFING (3 PLY).....480 SQ. YDS.  
PORTLAND CEMENT CONCRETE, ABUTMENTS AND RETAINING WALLS.....70 CU. YDS.  
PORTLAND CEMENT CONCRETE, PIERS.....150 CU. YDS.  
PORTLAND CEMENT CONCRETE, ROADWAY AND SIDEWALK SLABS  
ON STEEL BRIDGES.....240 CU. YDS.  
PORTLAND CEMENT.....690 BBL.  
BRIDGE DRAINAGE.....1 LUMP SUM  
STRUCTURAL STEEL, FABRICATED AND DELIVERED.....157,000 LBS.  
STRUCTURAL STEEL, ERECTION.....157,000 LBS.  
REINFORCING STEEL, DELIVERED.....64,800 LBS.  
REINFORCING STEEL, PLACING.....64,800 LBS.  
FRENCH DRAINS.....40 CU. YDS.  
ALUMINUM RAILING.....330 LIN. FT.



LOCATION MAP  
No Scale

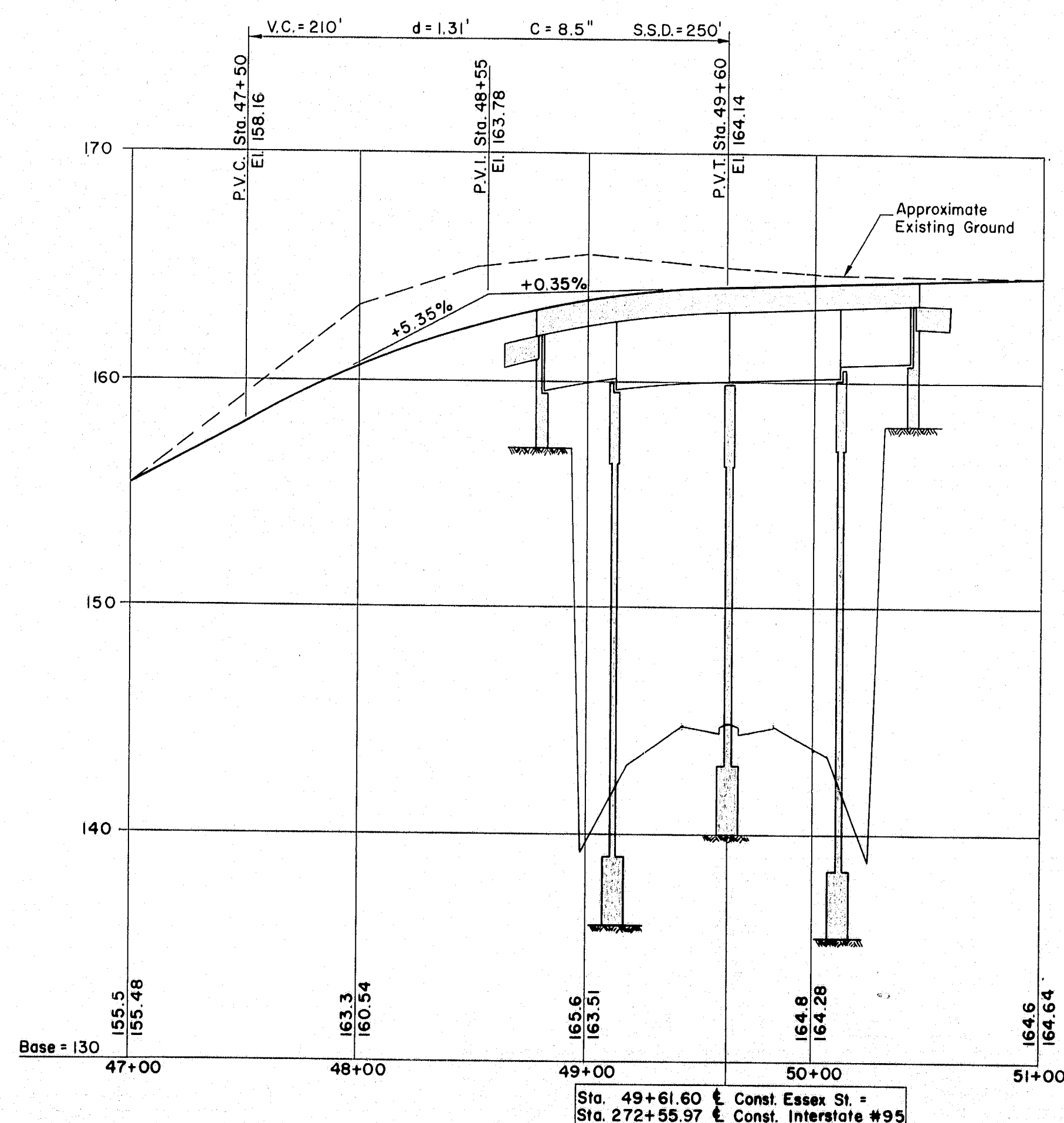


KEY PLAN  
Scale: 1" = 50'-0"



PROFILE ALONG INTERSTATE #95

Scale: Hor. 1" = 50'-0"  
Vert. 1" = 5'-0"



PROFILE ALONG ESSEX STREET

Scale: Hor. 1" = 50'-0"  
Vert. 1" = 5'-0"

APPROVED BY *Wm. A. Henderson* DATE 6-13-58

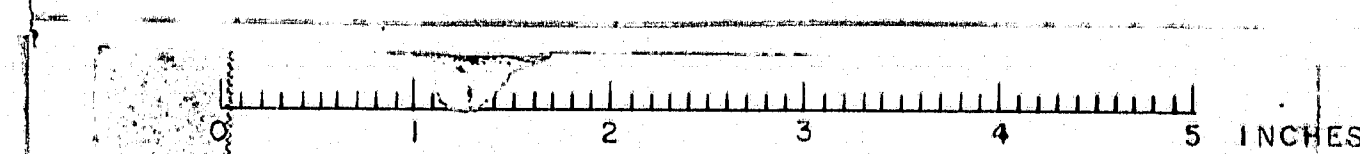
THE CLARKSON ENGINEERING CO., INC.  
CONSULTING ENGINEERS

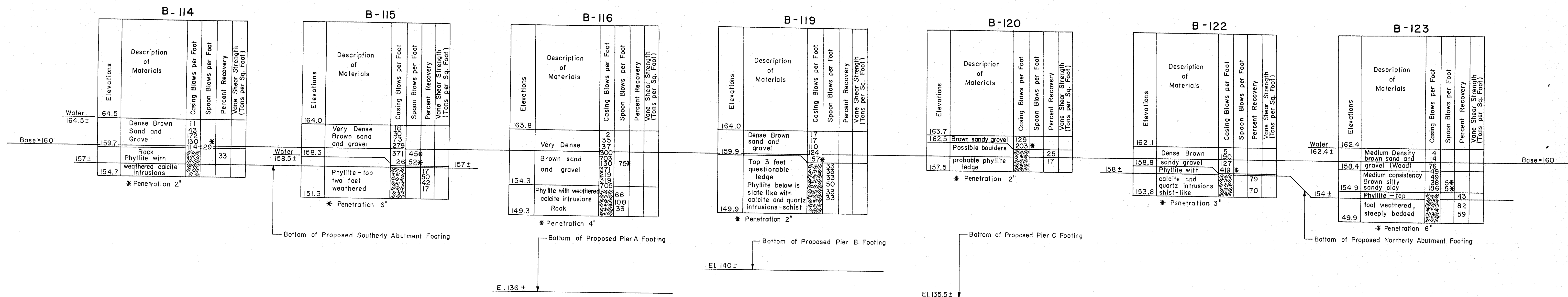
BOSTON

MASSACHUSETTS

SHEET 1 OF 7, AUGUSTA, MAINE

M-1200





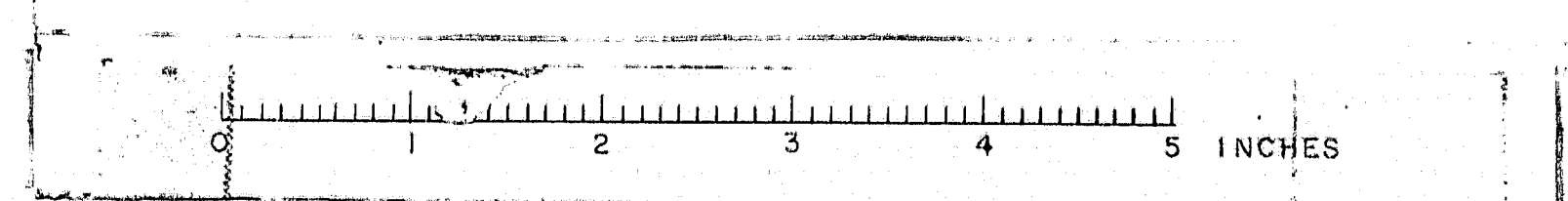
### BORING LOG

Scale: 1/8" = 1'-0"

- NOTES:
1. Location of borings are shown on the Key Plan thus: ● B-114
  2. Borings are taken for purpose of design and show condition at boring points only, but do not necessarily show nature of materials to be encountered during construction.
  3. The Contractor is to form his own opinion of the character of the materials and to make his own interpretation of the borings.
  4. The Engineer does not warrant the findings as being accurate or complete.
  5. Figures in boring columns indicate blows per foot on 2 1/2" casing or 1" spoon produced by a 356 # hammer with a fall of 14" and 12" respectively.
  6. Borings were taken by the Maine State Highway Commission during the month of July, 1957.

THE CLARKESON ENGINEERING CO., INC.		
DESIGN	CHECK H.P.	BRIDGE NO.
DRAWN P.W.G.	APPROVED WAH-CJM	SURVEY PLOT
STATE HIGHWAY COMMISSION		
INTERSTATE #95		
UNDER		
ESSEX STREET		
IN THE CITY OF		
BANGOR		
PENOBSCOT COUNTY		
BORING DATA		
SHEET 2 OF 7 AUGUSTA, MAINE		

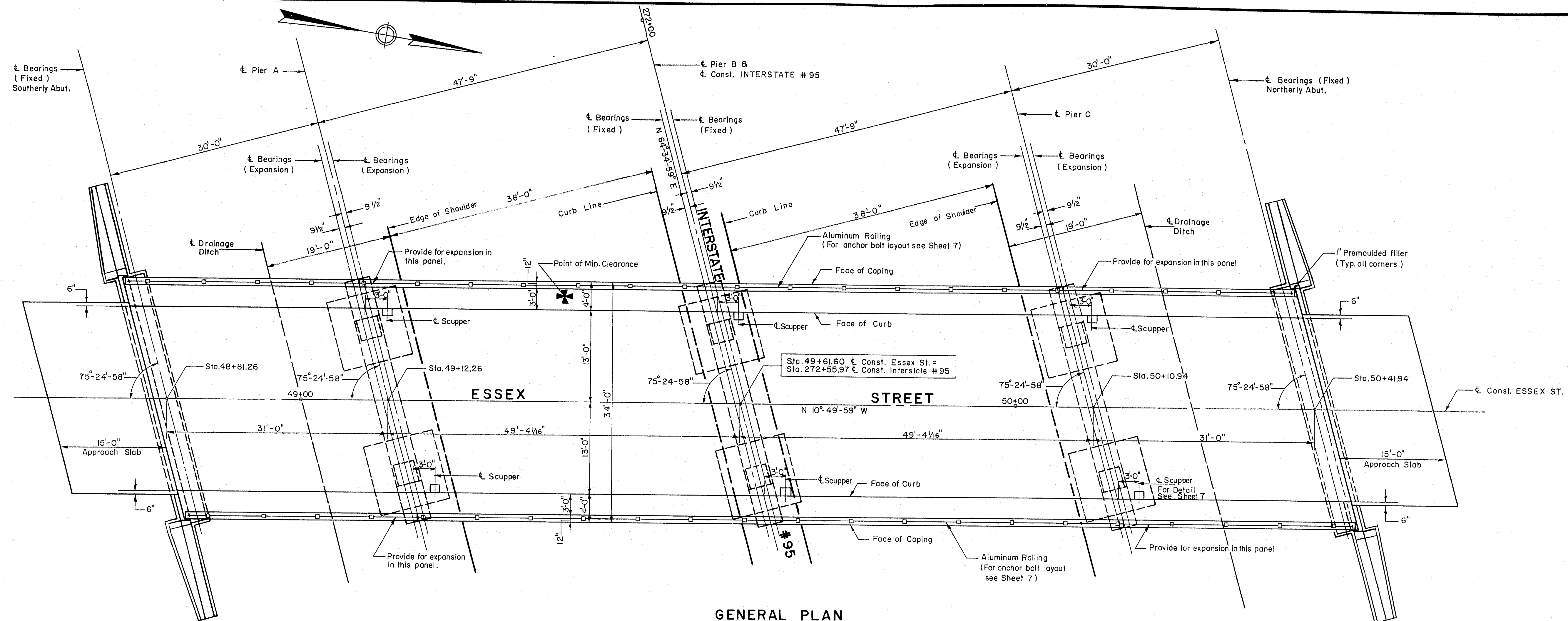
M-1201





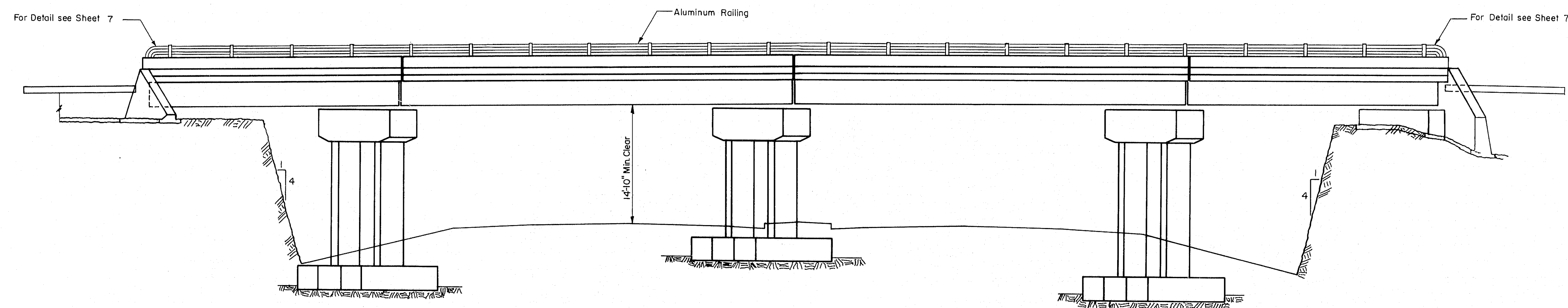
S.P.R. REG. NO.	STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	MAINE	I-95-8(5)179	14	18

# BANGOR INTERSTATE



GENERAL PLAN

Scale: 1/8" = 1'-0"



GENERAL ELEVATION

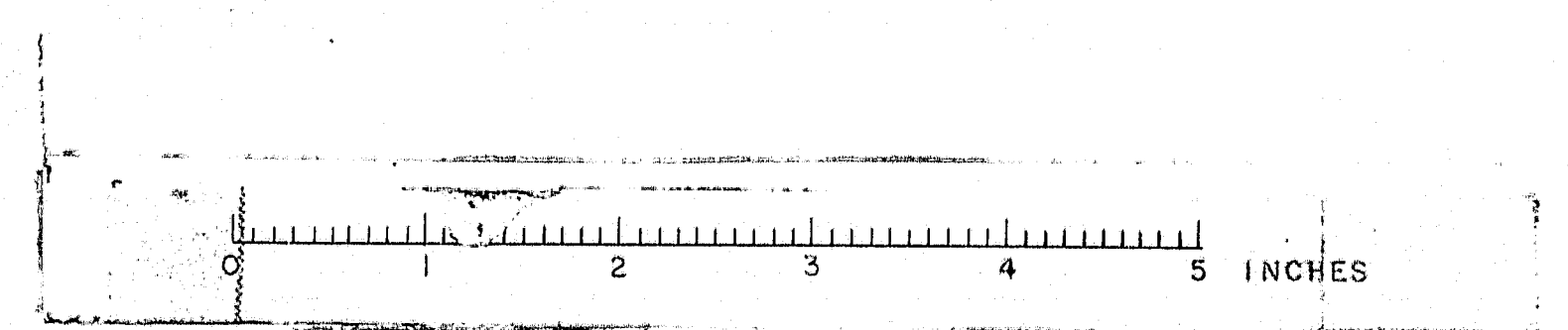
Scale: 1/8" = 1'-0"

THE CLARKESON ENGINEERING CO., INC.			
DESIGN	K.N.S.	CHECK PWG- H.P.	BRIDGE NO. SURVEY PLOT
DRAWN	E.K.	APPROVED C.J.M.	

STATE HIGHWAY COMMISSION  
INTERSTATE # 95  
UNDER  
ESSEX STREET  
IN THE CITY OF  
BANGOR  
PENOBSCOT COUNTY  
GENERAL PLAN & ELEVATION

SHEET 3 OF 7 AUGUSTA, MAINE

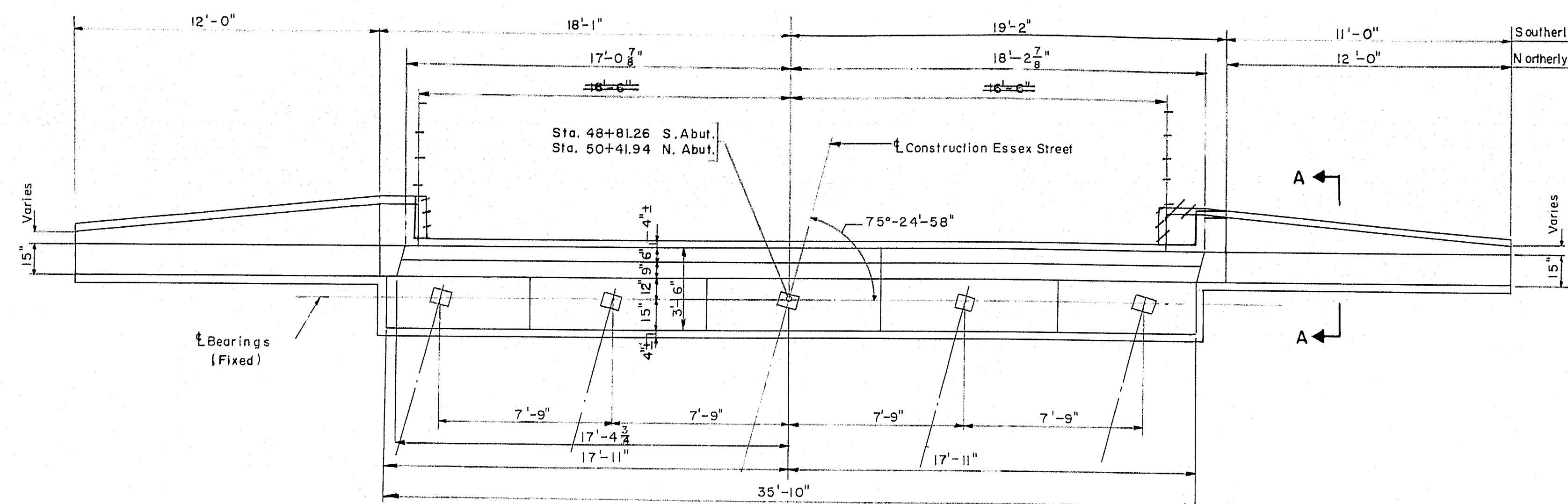
M-1202



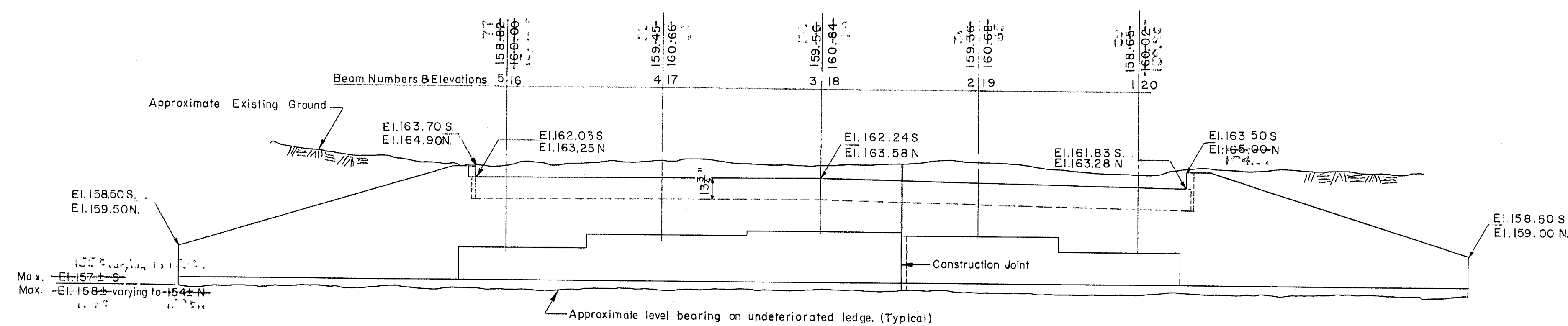


B.P.R. REG. NO.	STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	MAINE	1-95-8(5)179	15	18

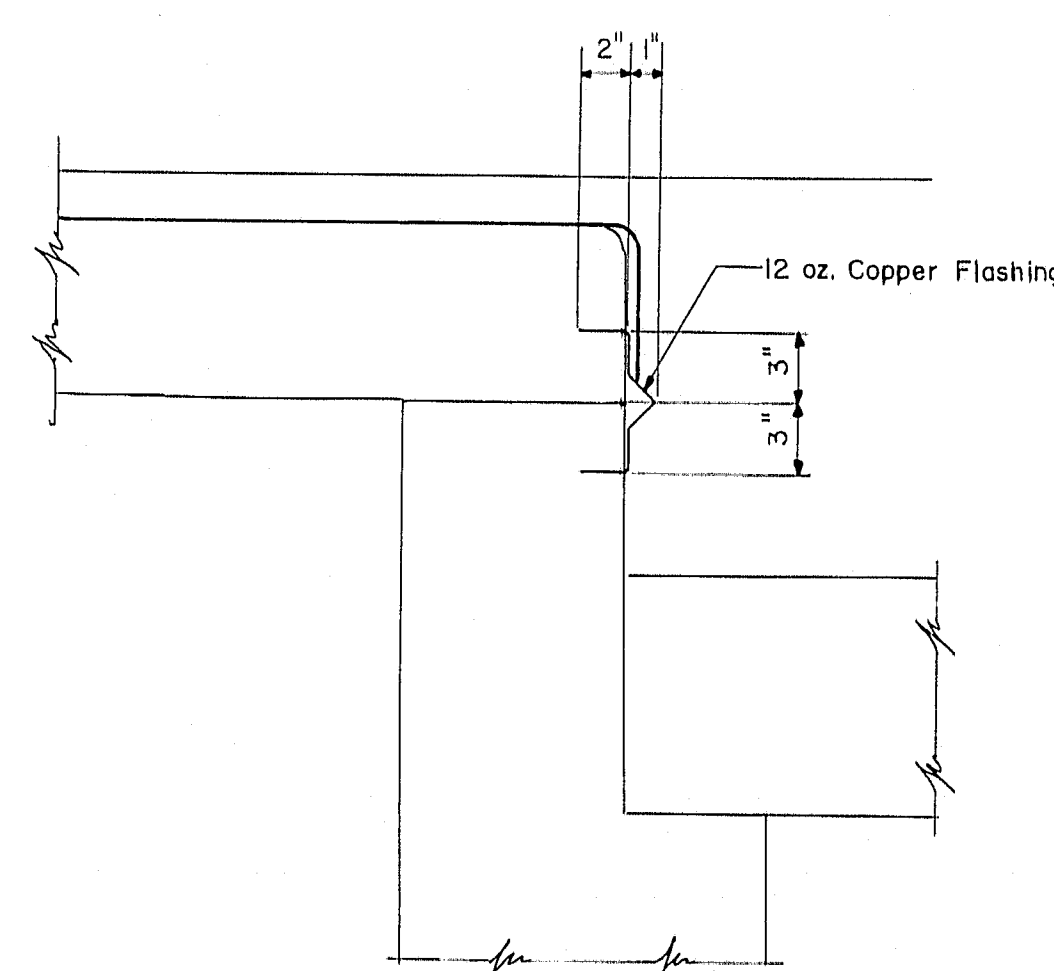
# BANGOR INTERSTATE



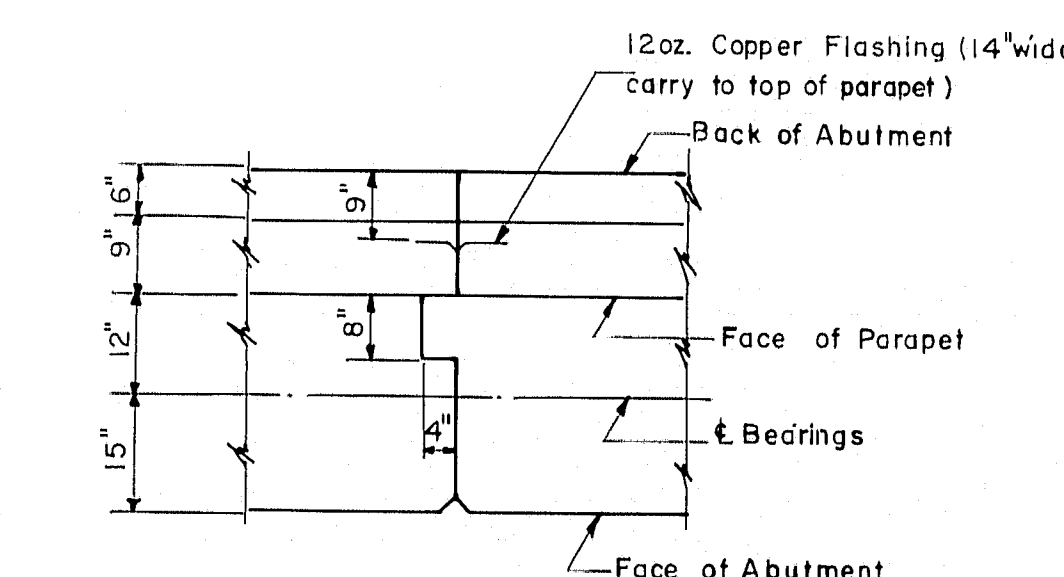
**SOUTHERLY ABUTMENT PLAN  
NORTHERLY ABUTMENT PLAN (SIMILAR)**  
Scale:  $\frac{1}{4}'' = 1'-0''$



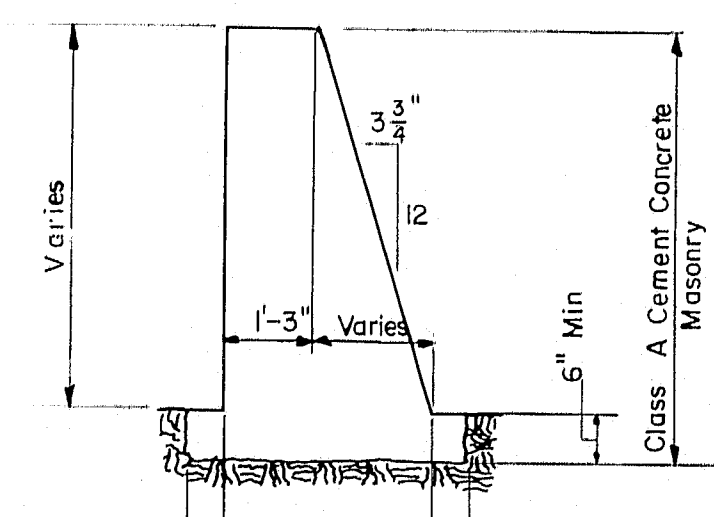
**SOUTHERLY ABUTMENT ELEVATION  
NORTHERLY ABUTMENT ELEVATION (SIMILAR)**  
Scale:  $\frac{1}{4}'' = 1'-0''$



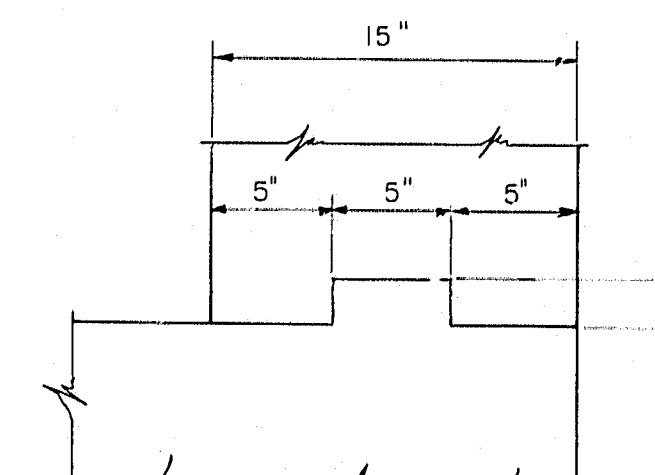
**DETAIL A**  
Scale:  $\frac{1}{2}'' = 1'-0''$



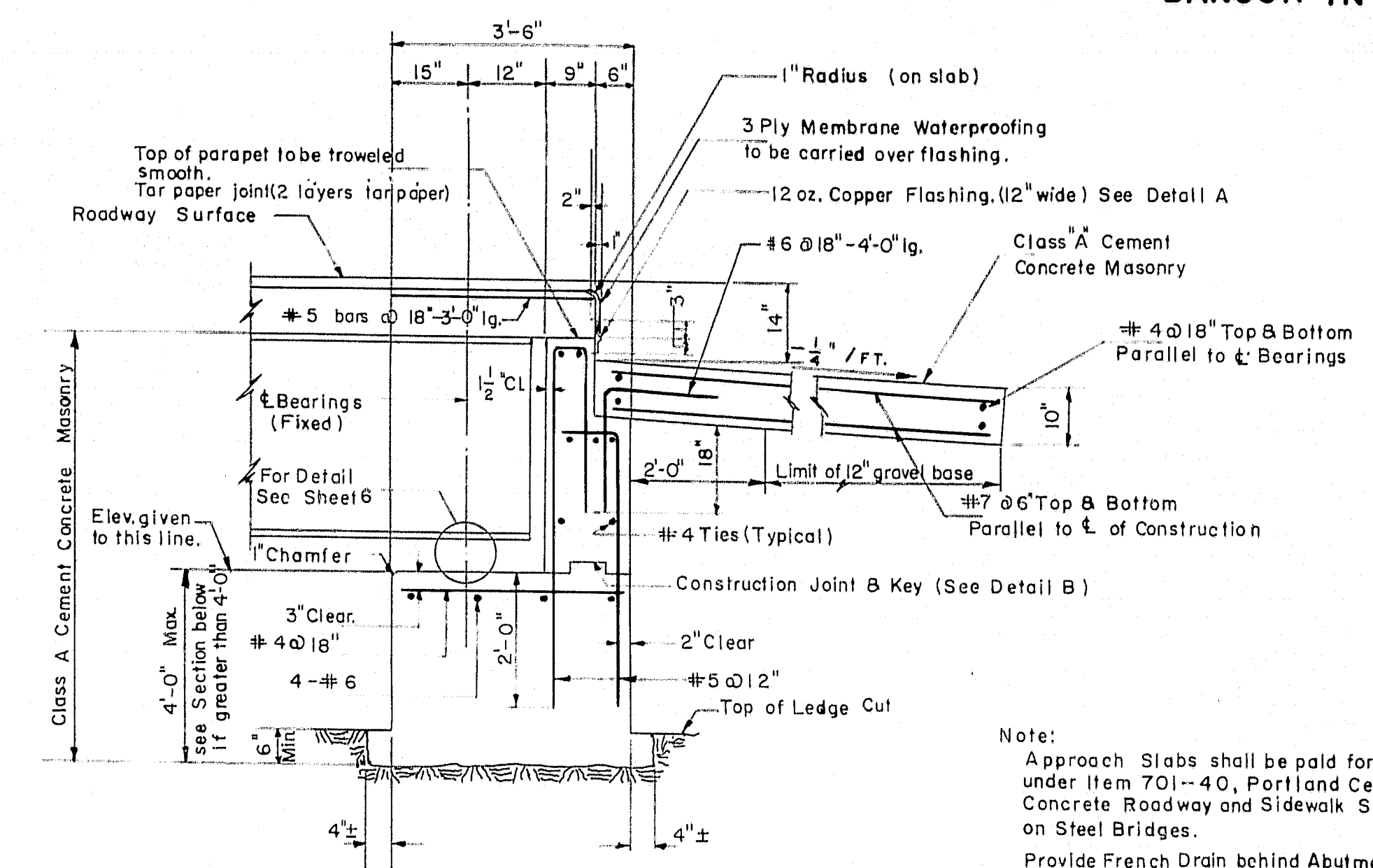
**ABUTMENT CONSTRUCTION JOINT**  
Reinforcement for run through Construction Joint  
Scale:  $\frac{1}{2}'' = 1'-0''$



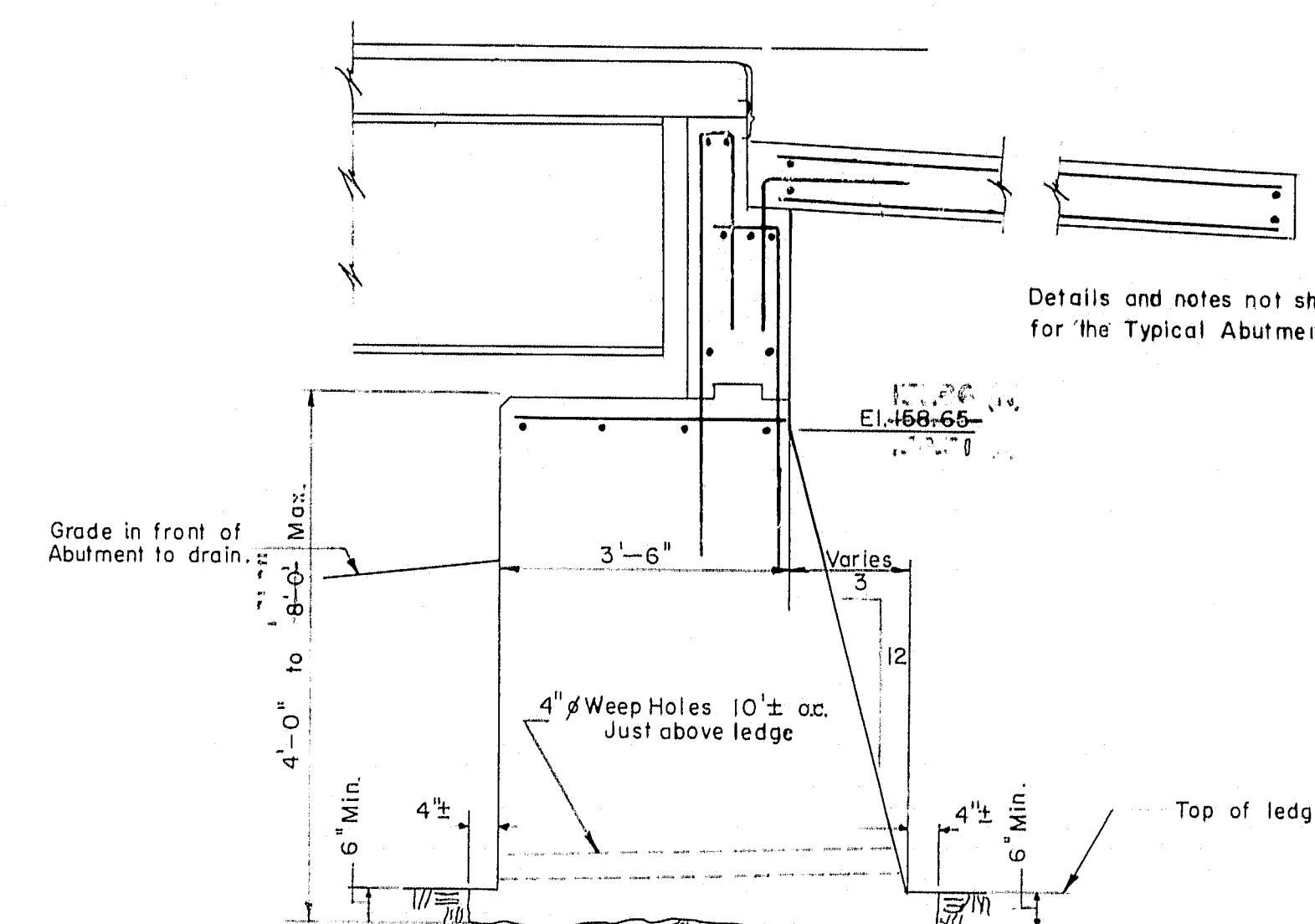
**SECTION A-A**  
Maximum Bearing Pressure 3700 #/sq. ft.  
Scale:  $\frac{3}{8}'' = 1'-0''$



**DETAIL B**  
Scale:  $\frac{1}{2}'' = 1'-0''$



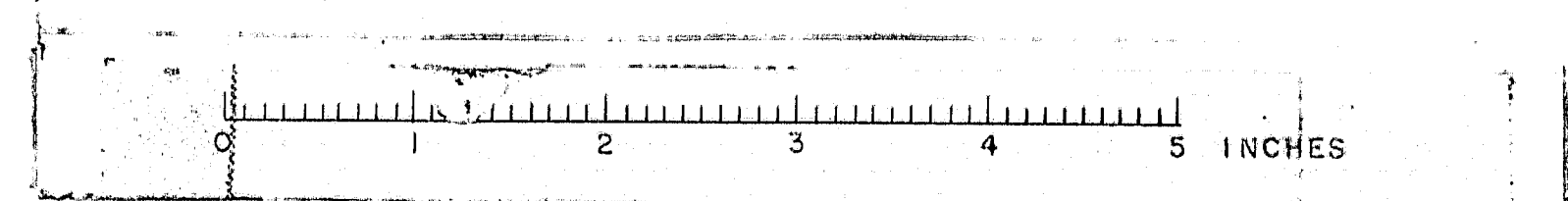
**TYPICAL ABUTMENT SECTION**  
Scale:  $\frac{1}{2}'' = 1'-0''$



**ALTERNATE ABUTMENT SECTION**  
Maximum Bearing Pressure 4500 #/sq. ft.  
Scale:  $\frac{1}{2}'' = 1'-0''$

THE CLARKESON ENGINEERING CO., INC.			
DESIGN	Y.E.	CHECK	R.F.B. H.R.
DRAWN	D.A.T.	APPROVED	WAH-CJM
BRIDGE NO. SURVEY PLOT.			
STATE HIGHWAY COMMISSION			
INTERSTATE #95			
UNDER			
ESSEX STREET			
IN THE CITY OF			
BANGOR			
PENOBSCOT COUNTY			
ABUTMENTS			
SHEET 4 OF 7 AUGUSTA, MAINE			

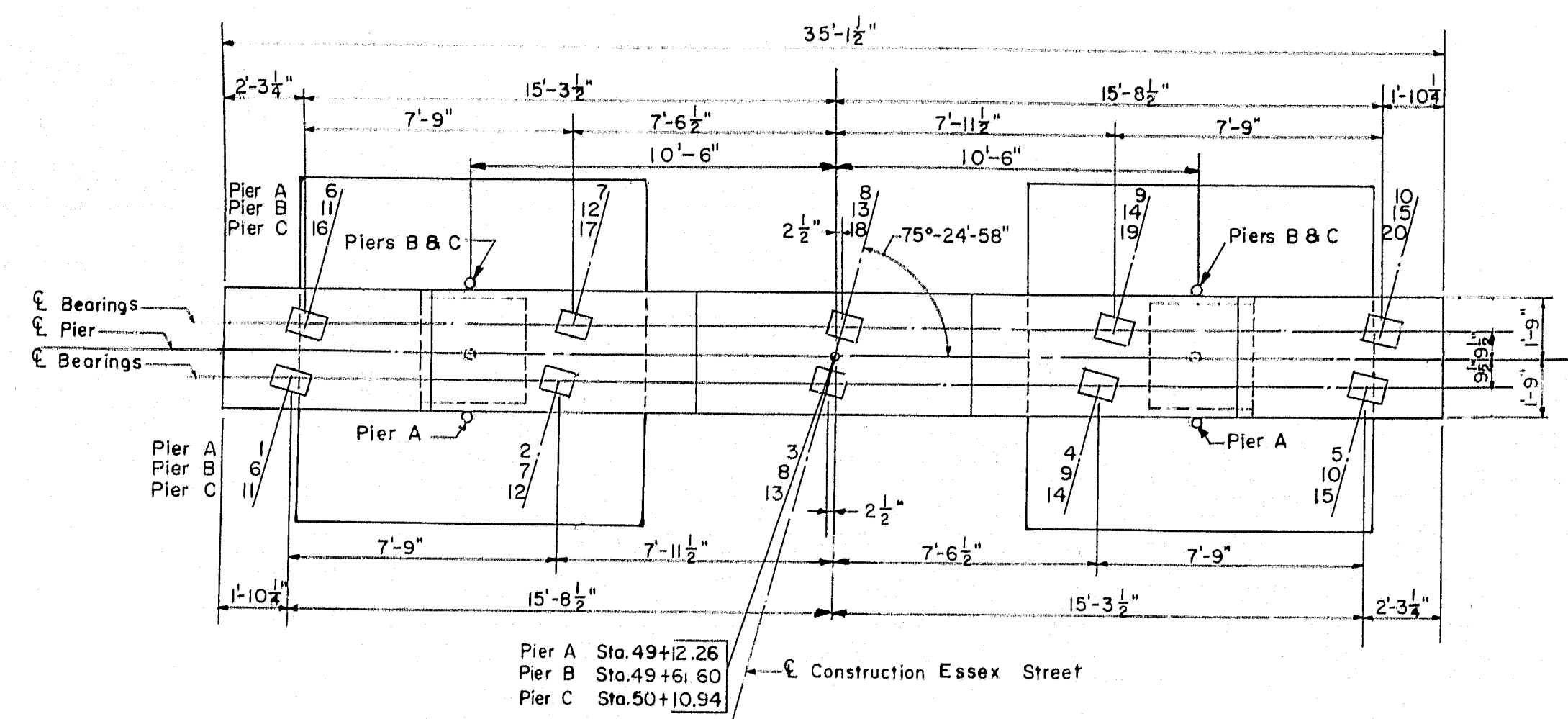
M-1203



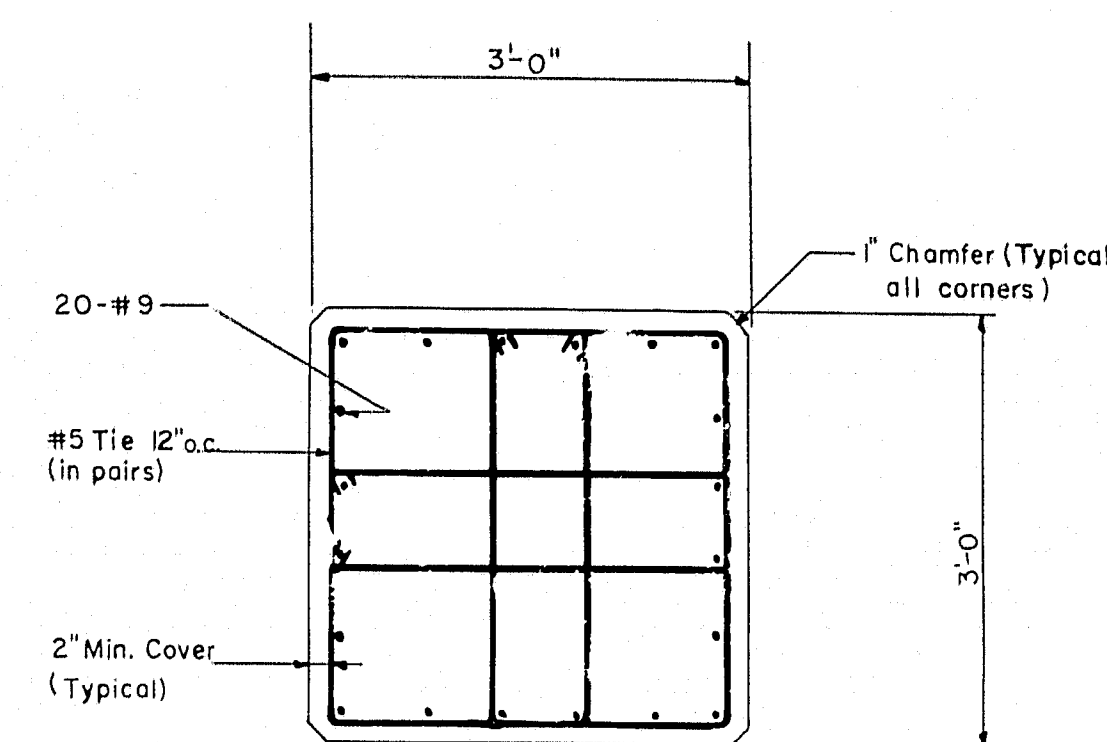


R.P.R. REG. NO.	STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	MAINE	1-95-B(5)179	16	18

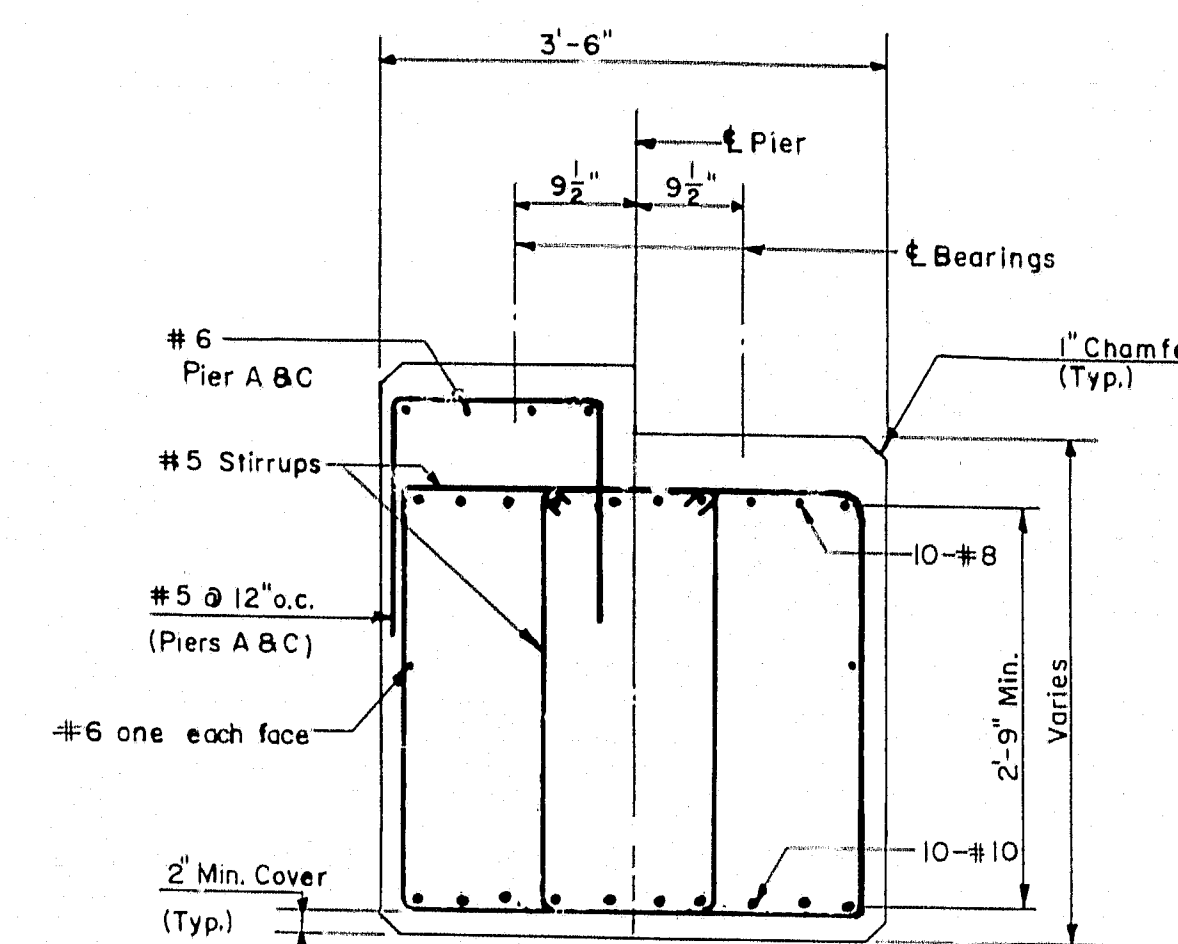
# BANGOR INTERSTATE



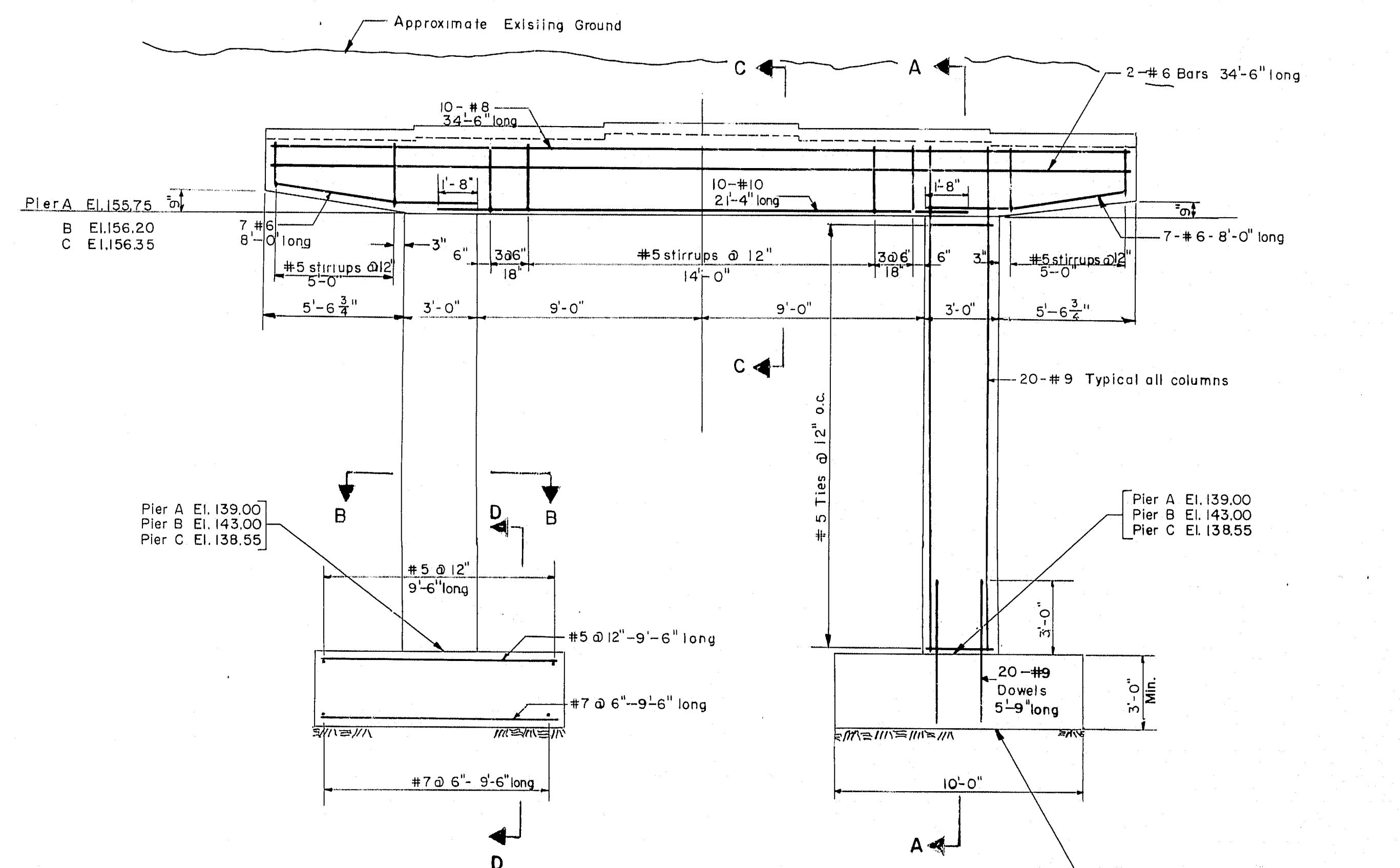
PLAN PIER A  
PLAN PIERS B & C (SIMILAR)  
Scale:  $\frac{1}{4} = 1'-0''$



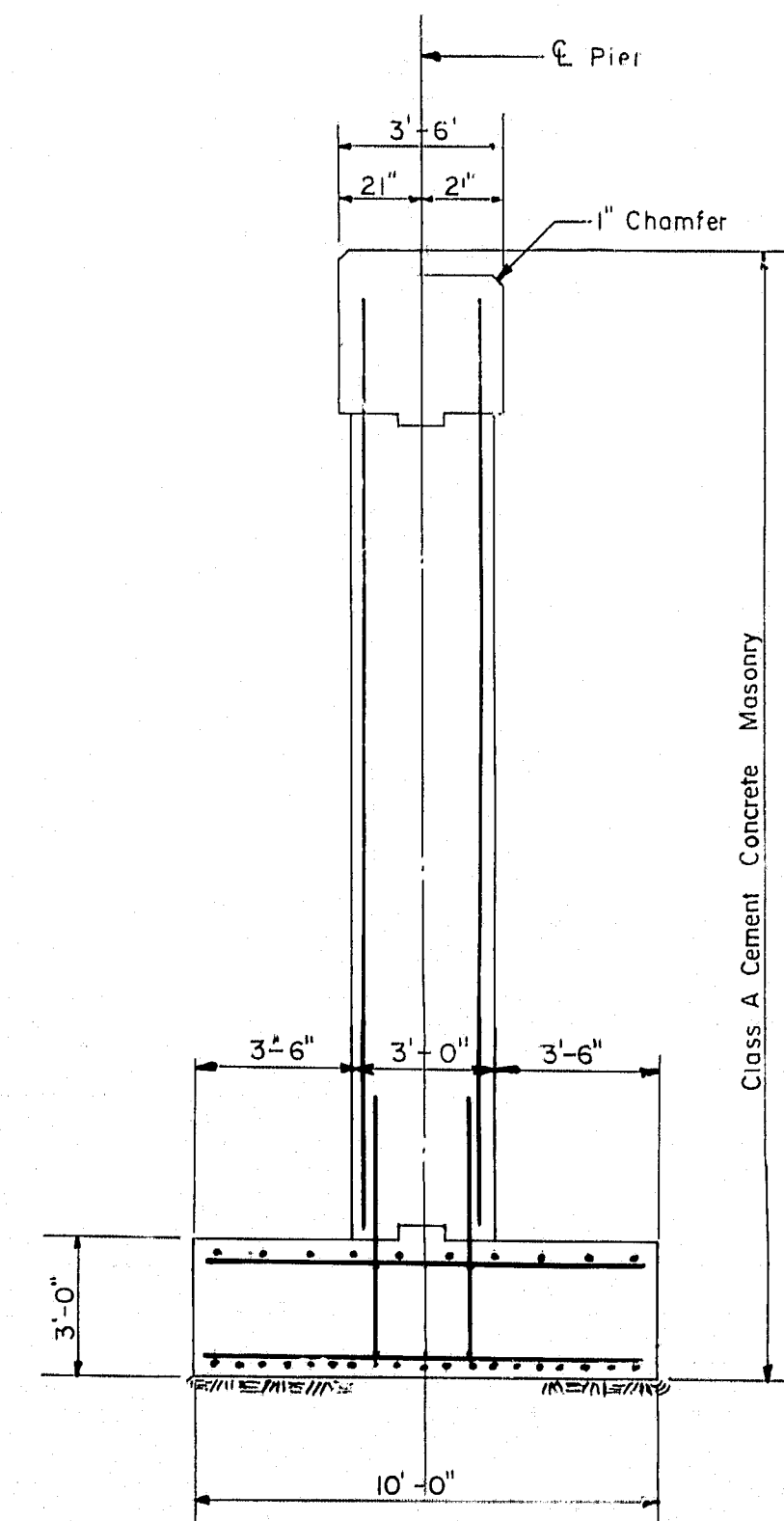
SECTION B-B  
Scale:  $\frac{3}{4} = 1'-0''$



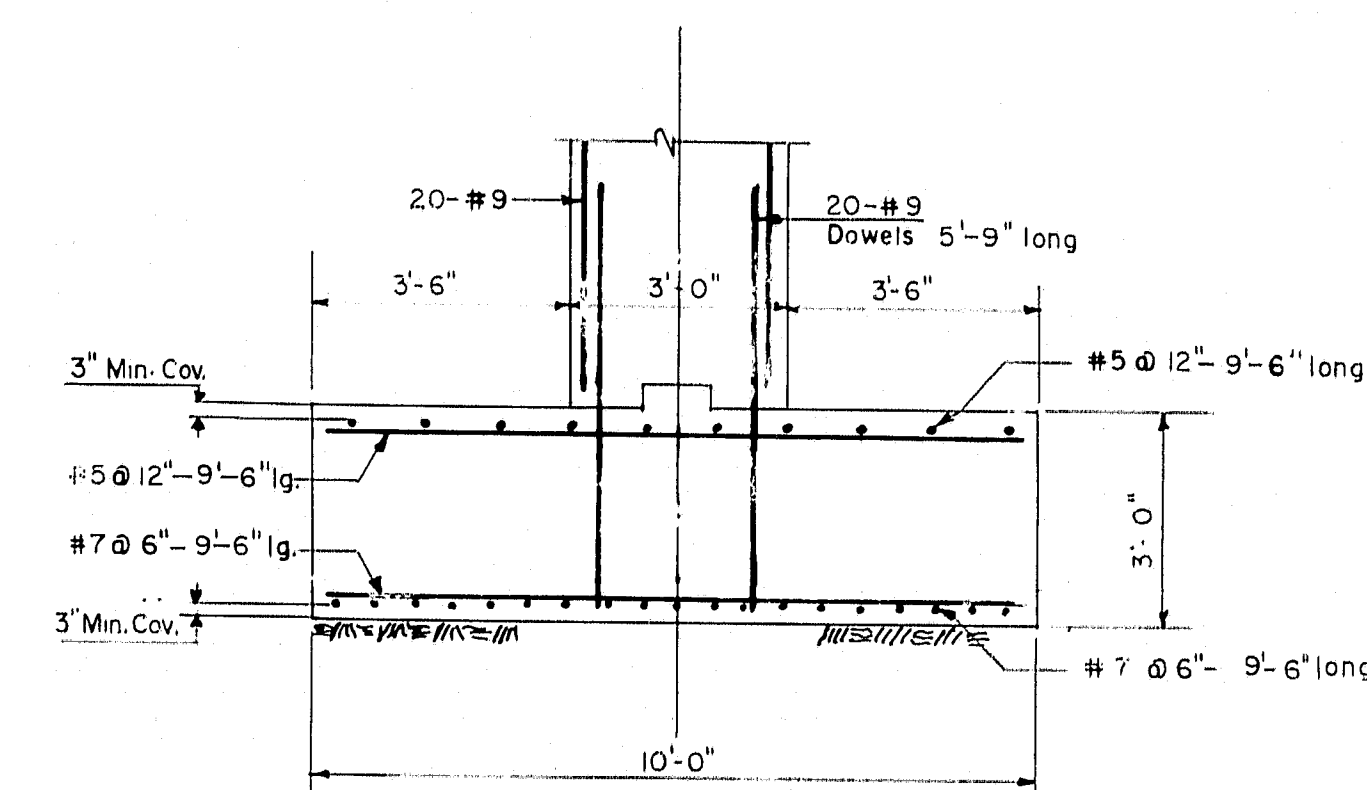
SECTION C-C  
Scale:  $\frac{3}{4} = 1'-0''$



ELEVATION PIER A  
ELEVATION PIERS B & C (SIMILAR)  
Scale:  $\frac{1}{4} = 1'-0''$



SECTION A-A  
Scale:  $\frac{1}{4} = 1'-0''$



SECTION D-D  
Scale:  $\frac{3}{8} = 1'-0''$

SCHEDULE of BEAM SEAT ELEVATIONS									
Beam No.	Pier A	Beam No.	Pier B	Beam No.	Pier C	Beam No.	Pier A	Beam No.	Pier B
1	159.24	159.26	6	159.73	159.69	11	159.86	159.89	16
2	159.93	159.45	7	159.90	159.85	12	160.02	160.56	17
3	160.12	159.64	8	160.05	160.02	13	160.18	160.72	18
4	160.00	159.50	9	159.91	159.87	14	160.04	160.57	19
5	159.35	159.38	10	159.76	159.72	15	159.68	159.92	20

SCHEDULE of BEAM SEAT ELEVATIONS									
Beam No.	Pier A	Beam No.	Pier B	Beam No.	Pier C	Beam No.	Pier A	Beam No.	Pier B
1	159.24	159.26	6	159.73	159.69	11	159.86	159.89	16
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3	160.12	159.64	8	160.05	160.02	13	160.18	160.72	18
4	160.00	159.50	9	159.91	159.87	14	160.04	160.57	19
5	159.35	159.38	10	159.76	159.72	15	159.68	159.92	20

## THE CLARKESON ENGINEERING CO., INC.

DESIGN Y.E. CHECK R.F.B. H.R. BRIDGE NO. SURVEY  
DRAWN F.R.D. APPROVED WAH.B.C.M. PLOT

## STATE HIGHWAY COMMISSION

## INTERSTATE #95

UNDIR

## ESSEX STREET

IN THE CITY OF

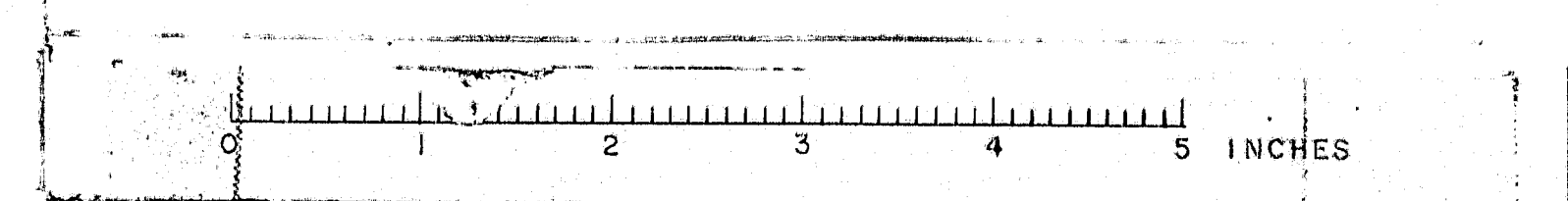
BANGOR

PENOBSCOT COUNTY

PIERS

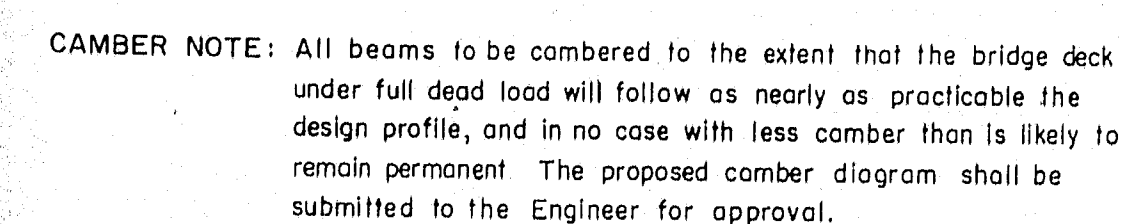
SHEET 5 OF 7 AUGUSTA, MAINE

M-1204





**BANGOR INTERSTATE**



Scale:  $\frac{1}{2}'' = 1'-0''$

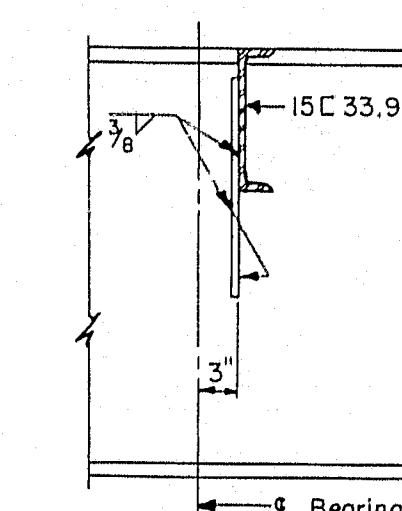
[illegible]

Scale:  $\frac{3}{4}$ " = 1' - 0"

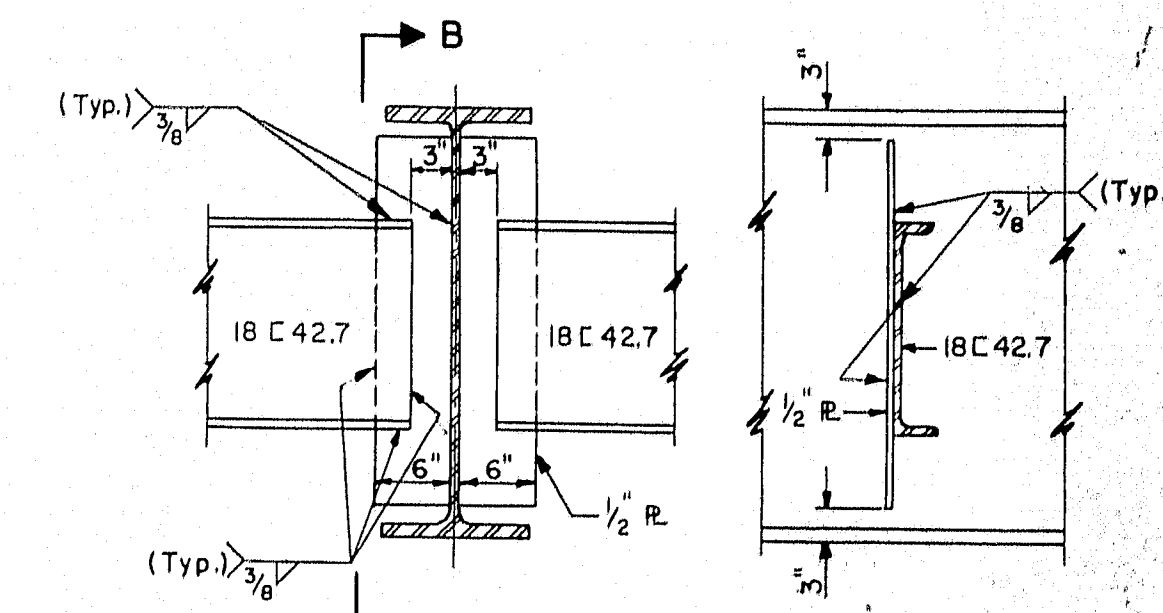


DETAIL A

Scale:  $\frac{3}{4}" = 1' - 0"$



SECTION A-A



Scale:  $\frac{3}{4}" = 1' - 0"$

Note: Welds to be shop or field welded



Scale:  $\frac{1}{2}'' = 1'-0''$

Top flange of stringers to be cut parallel to  $\epsilon$  of bearings.

Expansion Bearings:  
 Holes in sole R. & flange to be  $1\frac{3}{8} \times 2\frac{1}{2}$  slots.  
 Holes in masonry R. to be  $1\frac{3}{8}$   $\phi$ . Nuts to be hand tight.

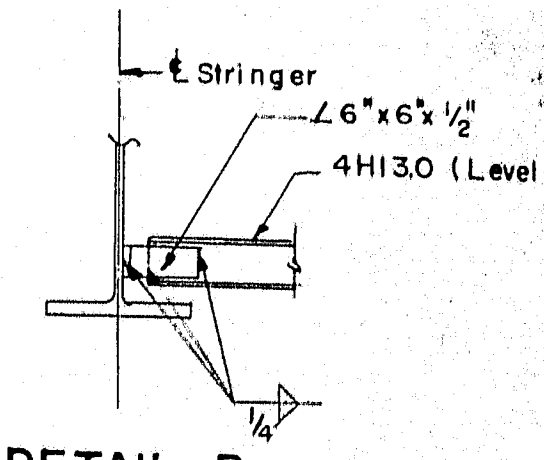
Fixed Bearings:  
 Holes in sole R., masonry R. & flange to be  $1\frac{3}{8}$   $\phi$ .

Dimensions and Labels:

- 8"  $\phi$  Abutrs
- $9\frac{1}{8}$   $\phi$  Piers
- $\epsilon$  Bearing
- 4" (two locations)
- Bridge Seat
- 1"  $\phi$  Sledge Bolts, with Washers & Hex Nuts
- Stringer
- $\frac{3}{8}$
- 6" — 30WF
- 8" — 36WF
- 12" — 30WF
- 14" — 36WF

All anchor bolts to be centered true before bearing plates are set in position.

Scale:  $1\frac{1}{2}" = 1' - 0"$



DETAIL B

DESIGN Y.E.	CHECK R.E. - HP	BRIDGE NO.
DRAWN PW.G.	APPROVED WAH-CJM	SURVEY PLOT

STATE HIGHWAY COMMISSION

INTERSTATE #95

UNDER  
ESSEX STREET

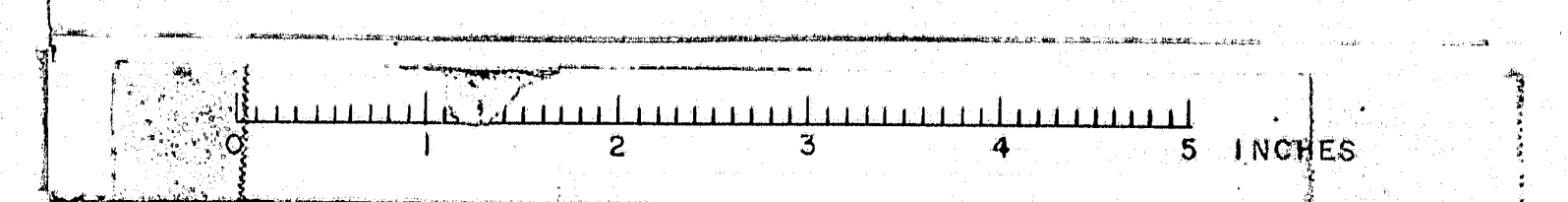
IN THE CITY OF

**BANGOR**

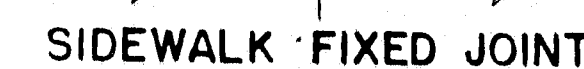
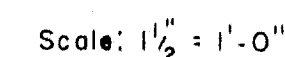
PENOBSCOT COUNTY

FRAMING PLAN &amp; DETAIL

SHEET 6 OF 7 AUGUSTA, MAINE M-1205



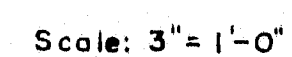




Note: Similar joint to be carried down face of curb to within 1" of roadway surface.



Note: Fixed detail similar except as shown in section.  
Scale:  $\frac{3}{4}" = 1' - 0"$



SECTION B - B



Scale:  $1\frac{1}{2}" = 1' - 0"$



Scale: 1" = 1' - 0"



Scale:  $\frac{1}{2}'' = 1' - 0''$